

GENERAL NOTES

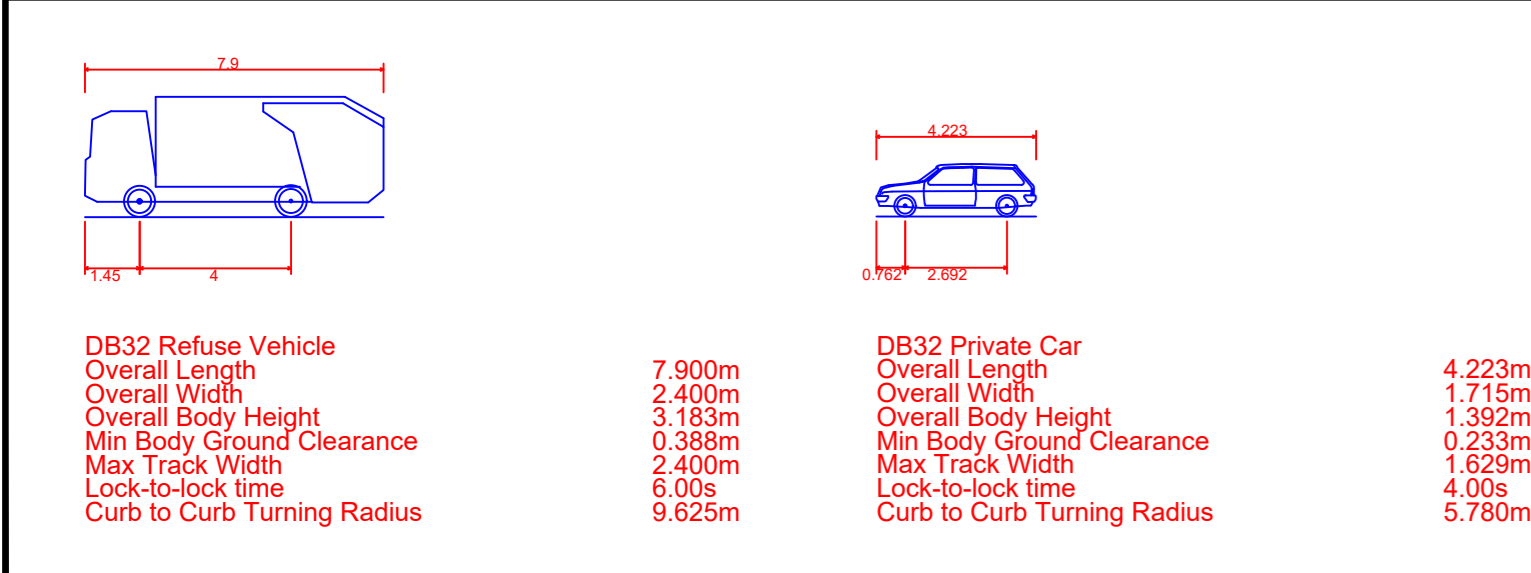
1. THE CONTRACTOR SHOULD READ THIS ROAD SPECIFICATION IN CONJUNCTION WITH THE RELEVANT TYPICAL DETAILS.
2. JOINTS BETWEEN NEW ROAD CONSTRUCTION AND EXISTING ROADS SHALL BE AS PER THE DETAILS IN THE SPECIFICATION. THE JOINTS IN THE EXISTING CARRIAGEWAY TO BE CUT BACK BY 1.5m WITH A ROTARY SAW TO FORM A VERTICAL FACE AND FRAMED IN ACCORDANCE WITH TRCC-SC030303. INCLUDE FOR ALL ADDITIONAL EXCAVATION AND FILLING TO ACHIEVE REQUIRED DEPTH OF SUB-BASE WHERE NEW AND EXISTING WORKS MEET.
3. ALL MANHOLES RAISED TO THE NEW ROAD LEVEL. WHERE REQUIRED, DRINKING CONCRETE TO MANHOLE COVERS AND FRAMES AND INCLUDE FOR SETTING FRAME IN CONCRETE TO NEW OR ADJUSTED LEVELS WHERE REQUIRED. PROTECT COVER AND FRAME DURING COURSE OF WORKS. ALL GULLIES TO MEET PROPOSED NEW SURFACE LEVEL WHERE REQUIRED.
4. FOOTPATH EXPANSION JOINTS SHALL BE NEATLY FORMED IN STRAIGHT LINES AT NOT GREATER THAN 3m CENTERS AND SO ARRANGED AS TO COINCIDE WITH THE JOINTS IN THE KERB JOINTS SHALL BE FORMED BY INSERTING A DOUBLE LAYER OF ROOFING FELT OR OTHER APPROVED METHODS WHICH SHALL EXTEND THE FULL DEPTH OF THE SLAB AND BE FINISHED OFF NEATLY AT THE SURFACE. THE CONTRACTOR SHALL ENSURE THE DOUBLE LAYER OF ROOFING FELT IS SUPPORTED IN THE JOINT AND HELD IN A STRAIGHT LINE DURING THE CONSTRUCTION PROCESS.
5. IN-SITU CONCRETE SHALL BE POURED ON A SUB-BASE OF 10mm NOMINAL THICK OF GRANULAR MATERIAL COMPLYING WITH CLAUSE 803. CONCRETE SHALL BE LAID AND COMPACTED IN COMPLIANCE WITH THE 800 SERIES OF THE SPECIFICATION FOR ROAD WORKS. ALL MATERIALS SPECIFIED SHALL COMPLY WITH REQUIREMENTS OF SR 21 (ANNEX A AMENDED TO I.S. EN 12620:2013) AND BASED ON THE REPORT OF PYRITE PANEL 2013 AGGREGATED FOR USE IN UNBOUND & HYDRAULICALLY BOUND GRANULAR MATERIALS.
6. THE VERTICAL ALIGNMENT OF THE FINISHED SURFACE SHALL NOT DEPART FROM THE DESIGN LEVEL BY MORE THAN +10mm AT ANY POINT. THE MAXIMUM DEVIATION OF THE SURFACE UNDER A STRAIGHT EDGE SHALL NOT BE GREATER THAN 5mm IN 3m. THE CONTRACTOR SHALL ALLOW FOR THE PROTECTION OF ALL EXISTING SERVICE CHAMBERS, MANHOLES AND DUCTING THROUGHOUT THE WORKS. ALL CONCRETE JOINTS AND EDGES SHALL BE BULL NOSED.
7. CURB TESTS SHALL BE CARRIED OUT ON THE SUBGRADE AT FORMATION LEVEL. THE RATE OF THE TESTS SHALL BE 1 TEST PER 50 LINEAR METERS OF ROAD. WHERE TEST VALUES VARY SIGNIFICANTLY ADDITIONAL TESTS MAY BE REQUIRED AT THE DISCRETION OF THE ENGINEER.
8. CAPPING LAYER SPECIFIED BASED ON ESTIMATED SUBGRADE CBR VALUE OF 8%. CAPPING LAYER MAY BE REDUCED/INCREASED SUBJECT TO ACTUAL SUBGRADE CBR TEST VALUES OBTAINED ON SITE.

TRAFFIC SITE LAYOUT LEGEND

- PROPOSED FOOTPATH
- PROPOSED ROAD
- PROPOSED GREENLANDSCAPE AREA
- PROPOSED TACTILE PAVING (UNCONTROLLED)
- PROPOSED LEVELS
- RUS 027- STOP SIGN

AUTOTRACK LEGEND

- VEHICLE WHEEL TRACK
- VEHICLE OVERWING



INFORMATION

REV. NO.	DATE	REVISION NOTE	DWN BY	CRD BY
P01	23/01/2024	ISSUED FOR PLANNING	PMC	MH

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CLIENT:	WESTMEATH COUNTY COUNCIL		
PROJECT:	PROPOSED SERVICED SITES KILLENBOYLEGAN, MOATE, CO. WESTMEATH		
TITLE:	TRAFFIC LAYOUT PLAN		
DRAWN:	CHECKED:	APPROVED:	JOB NO:
PMC	RD	MH	230894
DATE:	SCALE:	DRAWING NO:	REV:
14/07/2023	1:250	230894-ORS-Z2-00-DR-TR-700	P01

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